



STAFF REPORT

Planning and Economic Development Department

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KITSAP TRANSIT NORTH BASE MAINTENANCE FACILITY CONDITIONAL USE PERMIT AND SITE PLAN REVIEW

To: Heather Wright, Poulsbo Planning and Economic Development Director
From: Edie Berghoff, Senior Planner
Date: April 15, 2025
Subject: Kitsap Transit North Base Maintenance Facility Conditional Use Permit and Site Plan Review
 Type III Application

Planning and Economic Development (PED) staff respectfully recommend the approval of the Kitsap Transit North Base Maintenance Facility Conditional Use Permit and Site Plan Review, Planning File P-07-24-24-02.

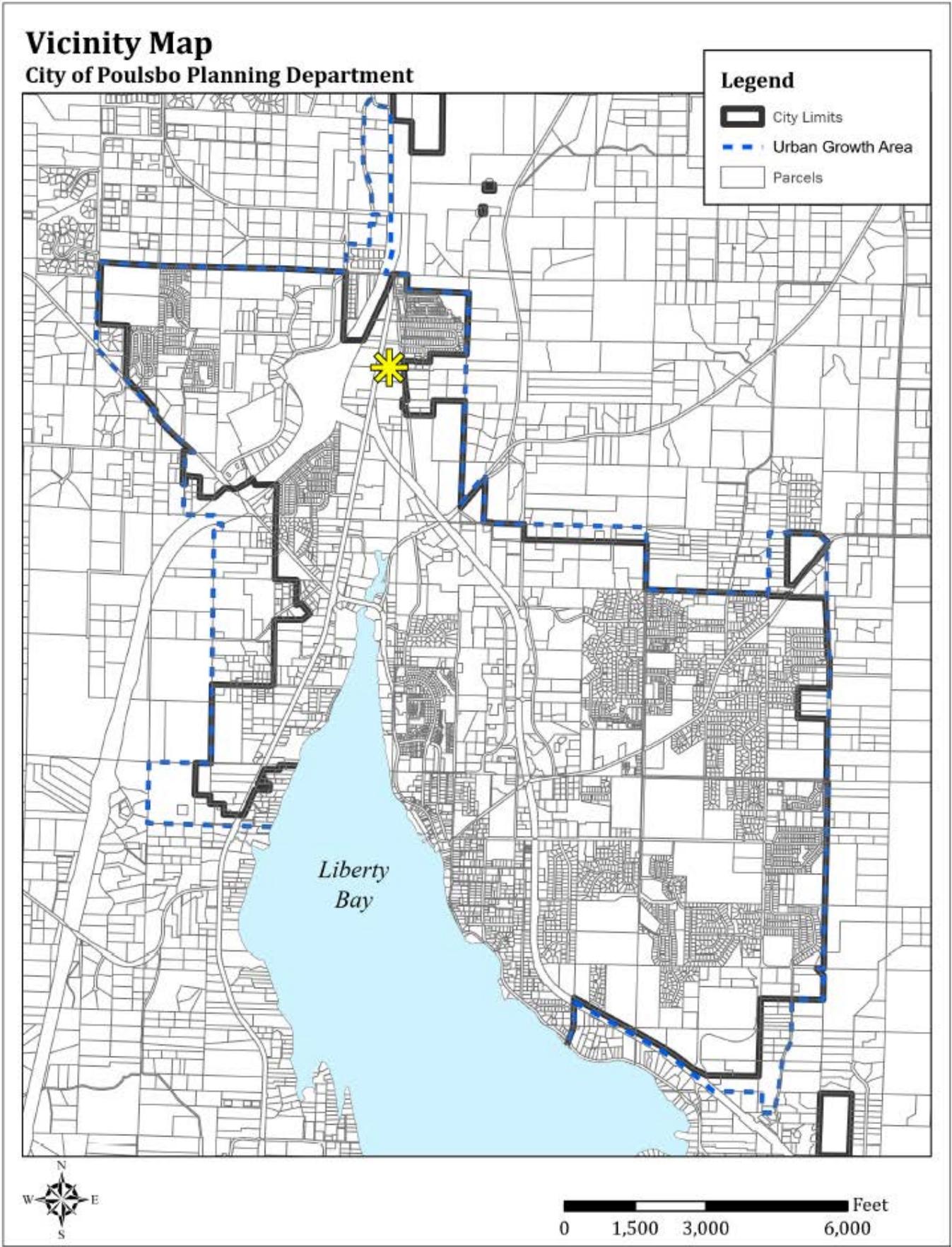
ACTION:

The Planning Commission shall hereby recommend (approval) (approval with modifications) (denial) of the, Planning File Kitsap Transit North Base Maintenance Facility Conditional Use Permit and Site Plan Review, Planning File P-07-24-24-02.

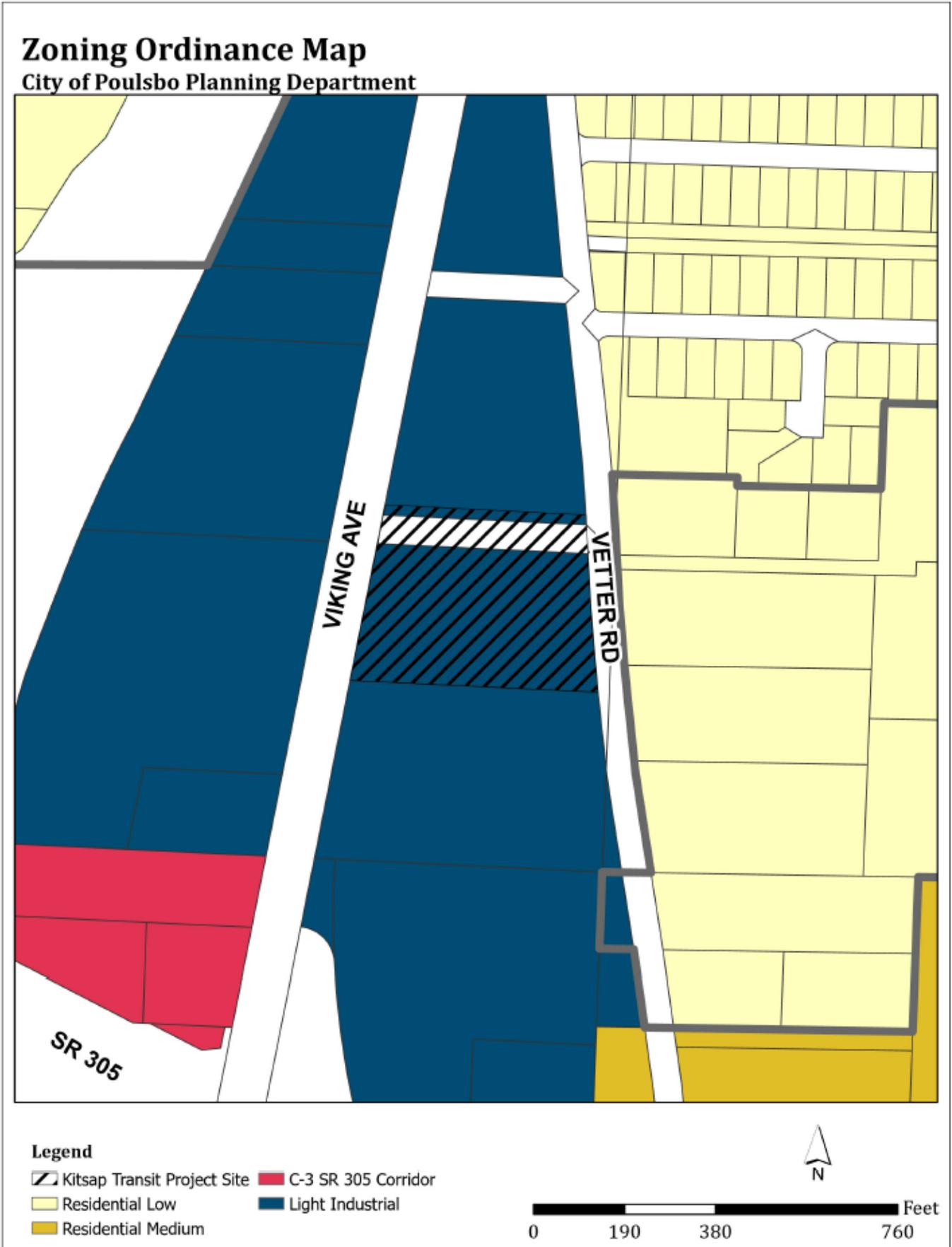
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**KITSAP TRANSIT NORTH BASE MAINTENANCE FACILITY CUP AND SPR
PLANNING FILE P-07-24-24-02**

I. GENERAL INFORMATION

Applicant/Agent Name and Address: Steffani Lillie, Service & Capital Development Director, Kitsap Transit, 60 Washington Avenue, Unit 200, Bremerton, WA 98337

Owner Name and Address: Kitsap Transit, 60 Washington Avenue, Unit 200, Bremerton, WA 98337

Land Use Review: Conditional Use Permit and Site Plan Review

Description of Proposal: Develop a new 15,400 square foot 5-bay maintenance facility, add 3,000 square foot office space and increase staff parking area. A 30 x 40 building and irrigation well in the southwest corner will be removed. All site paving will be replaced.

Site perimeter security will be a fence along the west and south property lines with automatic vehicle gates at Viking Avenue NW for bus access, at future NW Ruth Haines Road for staff access, and a wall along the east property line with gate breaks for emergency bus access, solid waste collection and person access to Vetter Road NE.

Location: 21711 and portion of 21710 Vetter Road | 102601-1-004-2007

Legal Description: THAT PORTION OF THE SOUTH HALF OF THE SOUTHEAST QUARTER OF THE NORTHEAST QUARTER, SECTION 10, TOWNSHIP 26 NORTH, RANGE 1 EAST, W.M., IN KITSAP COUNTY, WASHINGTON, LYING EAST OF EAST MARGIN OF STATE HIGHWAY NO. 21; EXCEPT THE SOUTH 375 FEET THEREOF; AND EXCEPT VETTER COUNTY ROAD; ALSO EXCEPT THAT PORTION CONVEYED TO STATE OF WASHINGTON FOR ROADS BY DEED RECORDED MAY 31, 1951, UNDER AUDITOR'S FILE NO. 536534.

And south portion of PARCEL B OF BOUNDARY LINE ADJUSTMENT RECORDED UNDER AF# 200309170138, BEING A PORTION OF THE FOLLOWING DESCRIBED PROPERTY: >>PARCEL 2 OF SHORT PLAT SUBDIVISION NO. 2690 PER AF 9005110157 DAF >> THE SOUTH 525 FEET AS MEASURED ALONG THE WEST LINE THEREOF OF THE FOLLOWING DESCRIBED TRACT: THAT PORTION OF THE NORTHEAST QUARTER OF SECTION 10, TOWNSHIP 26 NORTH, RANGE 1 EAST, W.M., IN KITSAP COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 10; THENCE SOUTH 2*14'16 WEST 994.23 FEET TO THE SOUTHEAST CORNER OF POULSBO HEIGHTS, AS PER PLAT RECORDED IN VOLUME 3 OF PLATS, PAGE 130, RECORDS OF KITSAP COUNTY, WASHINGTON; THENCE NORTH 87*12'39 WEST 185.98 FEET ALONG THE SOUTH LINE OF SAID PLAT TO THE TRUE POINT OF BEGINNING; THENCE CONTINUE NORTH 87*12'39 WEST 151.75 FEET TO THE EAST MARGIN OF STATE HIGHWAY SR 3; THENCE SOUTH 11*18'23 WEST ALONG SAID MARGIN 944.94 FEET TO THE NORTH MARGIN OF VETTER COUNTY ROAD; THENCE SOUTH 87*15'02 EAST ALONG THE NORTH MARGIN THEREOF 425.33 FEET OF A CORNER IN SAID ROAD; THENCE NORTHWESTERLY ALONG THE WEST MARGIN THEREOF, 943.85 FEET TO THE TRUE POINT OF BEGINNING (ALSO KNOWN AS PARCEL B,C, AND GREEN BELT EASEMENT OF SHORT PLAT NO. 2690 UNDER AUDITOR'S FILE NO. 8106290104.)>>>>EXCEPT THAT PORTION CONVEYED TO THE CITY OF POULSBO FOR COOPERATION WAY UNDER AUDITOR'S FILE NO. 200702260176, RECORDS OF KITSAP COUNTY, WASHINGTON.

And including NE Vetter Road right-of-way identified in Ordinance 2009-07 to be vacated.

Comprehensive Plan and Zoning Designation:

Site: Light Industrial
 North: Light Industrial
 South: Light Industrial
 East: Low Density Residential
 West: Light Industrial

Existing Land Use:

Site: Kitsap Transit Office
 North: Kitsap Transit Bus Parking Facility
 South: Kit Transit Park and Ride
 East: Single Family Residential
 West: City of Poulsbo Public Works Facility

Site Features: The site is made up of the Kitsap Transit North Base operations/office property, NE Vetter Road pending right-of-way vacation completion, and a few feet/portion of the bus parking property. West half of the site includes an operations/driver day use building, storage building, associated parking for office/driver use and an irrigation well. Landscaping surrounds developed area and landscaping islands are located in parking area. Water and sewer serve the site. The undeveloped east half cover is mowed grass field with areas of trees. Created rock lined drainage surrounded by trees and shrubs occupies the center of the undeveloped area. Fill has been used on the site.

The south 40 feet of the property is developed roadway for future NW Ruth Haines Road which will be become dedicated right-of-way when NW Vetter Road vacation is completed.

Aerial Photograph of Subject Site: Property lines are shown in orange. Area of development outlined in red.



History of the Site: Two structures were built on the site in 1989 with commercial use. A variety of tenants including teachers supply store, home alarm company and glass studio occupied the two buildings prior to transit occupying the site.

The site was annexed to Poulsbo in 2004 in the North Viking Avenue UGA Annexation (Ordinance 2004-21). County records indicate Kitsap Transit purchased the property in 2007. The site was a construction office during development of the bus “barn” to the north and park-n-ride to the south.

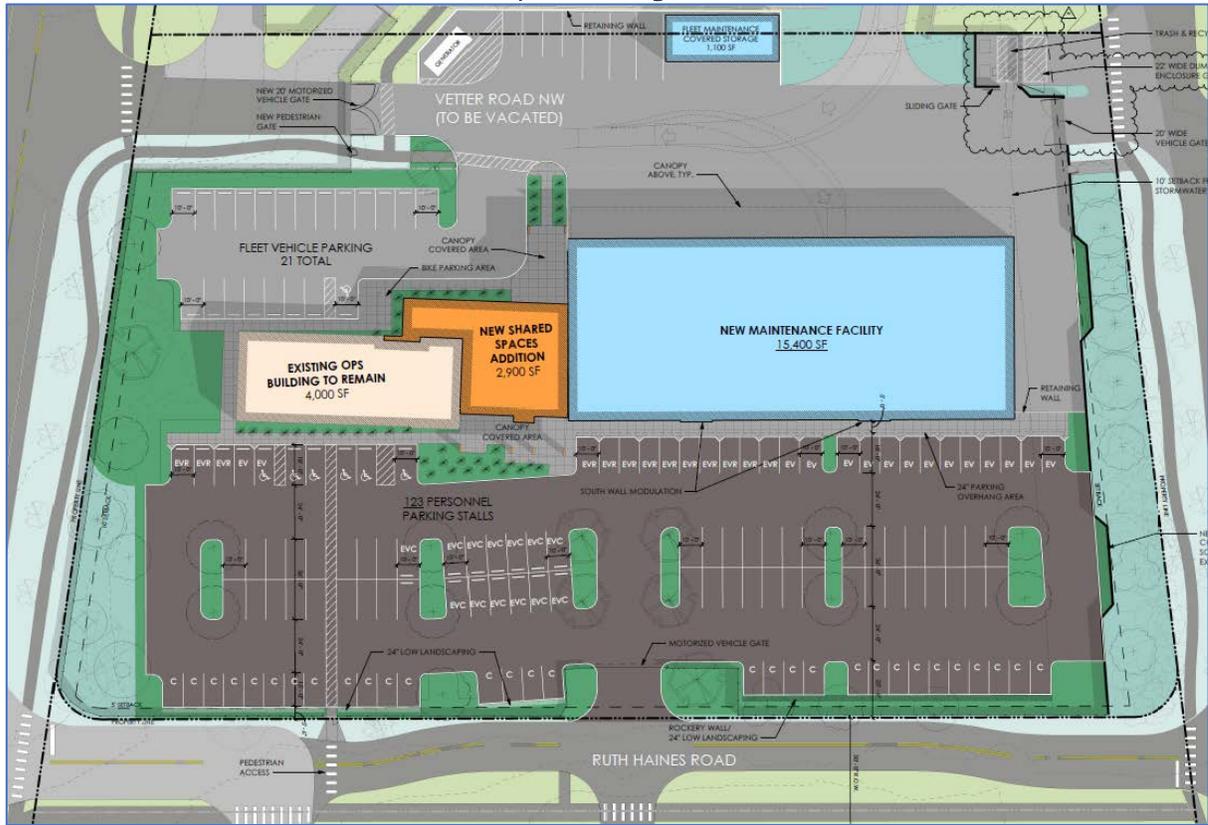
In 2009 Kitsap Transit requested NE Vetter Road be vacated between Viking Avenue NW and Vetter Road NE, north of the proposed maintenance shop. City Council approved Ordinance 2009-07 requiring construction and dedication of NW Ruth Haines Road located south of the proposed maintenance shop. Completion of the project is pending.

Septic and one of two wells were decommissioned in 2011 and connections to city water and sewer made. The remaining well was converted to irrigation for the property. Kitsap Transit fully occupied the site.

Kitsap Transit North Base & Park and Ride Site Plan and Conditional Use Permit was approved by the Hearing Examiner in November 2013 (Planning File 02-15-12-1) for bus parking facility north and Park and Ride south. Minor modifications were approved by the Planning Director in August 2014 (Planning File 07-28-14-1). Minor remodels of the office building have been completed under various building permits.

Site Drawing:

CUP/SPR Drawing Exhibit C



II. APPLICABLE REVIEW CRITERIA

This application is subject to review under PMC 16.20 Critical Areas Ordinance, PMC 18.90 Business and Employment Districts, PMC 18.120 Design Review; PMC 18.130 Landscaping; 18.140 Off-Street Parking and Loading; 18.230 Conditional Use Permits; PMC 18.270 Site Plan Review; and Title 19 PMC Project Permit Application Procedures.

III. PMC 16.20 CRITICAL AREAS ORDINANCE

Wetland

No wetlands are identified on or within 300 feet of the project site. The nearest identified wetland is more than ¼ mile (1,320 feet) south of the site. The wetland is located on the Oslo Bay Apartments property which is currently under development. In the Oslo Bay Apartments staff report, Exhibit H.3, Habitat Management Plan, the wetland is described as a Category IV, 0.10-acre, slope, forested, saturated only, wetland with permanently flowing stream.

Stream

No streams are identified on or within 300 feet of the project site. The site is located north of an unnamed tributary of Dogfish Creek, which flows into Liberty Bay. The stream begins approximately 400 feet south of the project site, flowing south roughly 700 feet before becoming a Type F stream. (Exhibit J Critical Area Assessment, Appendix C). A Category IV wetland with permanently flowing stream is an additional 400 feet south of the stream type break. The stream flows through the wetland for approximately 250 feet before entering a culvert under State Highway 305.

Stormwater from the development area is collected in a vault located in the parking area south of the maintenance building. Treated stormwater is directed through the NW Ruth Haines Road system and through a stormwater pond constructed with the Park & Ride before exiting to the Type N stream. The mapped streamside wetland is more than ¼ mile downstream.

Critical Aquifer Recharge Area

Poulsbo Aquifer Critical Area Map identifies a portion of the site as potential aquifer recharge area of concern due to potential shallow aquifer and possible soils with high infiltration potential. The site is in a Kitsap County mapped wellhead protection zone for the Ashley Water System located approximately 1.3 miles southeast of Kitsap Transit property.

The United States Department of Agriculture (USDA) Natural Resources Conservation Service (NRCS) Web Soil Survey Map identifies the site is approximately 15% Norma fine sandy loam (map unit symbol 37) and the remaining approximately 85% Poulsbo gravelly sandy loam, 0 to 6 percent slopes (map unit symbol 39). PMC Table 16.20.510.A identifies Norma soil 37 may have relatively high permeability and high infiltration potential.

Land uses in Table 16.20.515 require a special report. The proposal is expansion of the existing operations building and adds a maintenance facility for transit vehicles. The use is not identified in PMC Table 16.20.515, therefore a special report is not required.

Infiltration is identified as infeasible on the site due to shallow depth to intact glacial till. The Hydrogeologic Assessment identifies the site is underlain by approximately 200 feet thickness of confining units which limit vertical recharge to the underlying sea-level aquifer stating that in their opinion redevelopment at the “site will have minimal impact on groundwater quantity and quality so long as the site redevelopment is designed, constructed, operated and maintained in conformance with appropriate BMPs and regulation.” (Exhibit M).

IV. PMC 18.90 BUSINESS AND EMPLOYMENT DISTRICTS

The city’s comprehensive plan establishes three land use designations that support the business and employment goals and policies of the city of Poulsbo. Office commercial industrial (OCI), business park (BP) and light industrial (LI) are the land use designations.

Staff Comment. The property is designated LI.

A. 18.90.030 Uses.

Permitted, conditional and administrative conditional use, and prohibited uses are identified in Table 18.90.030.

Staff Comment. Transit facilities, including park and ride lots and transfer centers, are permitted as a conditional use in the LI zone.

B. 18.90.040 Development standards.

Table 18.90.040 identifies development standards.

Table 18.90.040 Business and Employment Districts Development Standards	
Minimum lot area	No minimum lot area requirement
Maximum building coverage	As provided after setbacks, landscaping, parking and other applicable standards are met.
Minimum setbacks when lot line is adjacent to a R zoning district	20’, plus an additional 1 foot for each foot the building wall facing the R district exceeds an average of 35’ in height, to a maximum setback requirement of 20’.
Minimum setbacks when lot line is adjacent to nonresidential zoning district	Front yard: 10’ Other yards: 5’ Street (public or private) frontage: 10’ Setbacks may be enlarged to provide additional area to meet overall site landscaping requirement.
Maximum building height ¹	No building or structure shall exceed 35’ in height; provided, that the height limit in area 1 (as shown below) shall be 45’; and the height limit in area 2 (as shown below) shall be 55’.
¹ See Section 18.150.050 for building height measurement and Section 18.310.010 for building height exceptions.	

Staff Comment. Proposal is consistent with standards identified in PMC Table 18.90.040.

Setbacks:

- The site is adjacent to residential zone to the east. A setback of 20 feet at the north and 30 feet at the south is provided. Setback is measured from property line to security/sound wall and curbing.

- Front setback, measured at Viking Avenue NW, exceeds 10 feet. Setback is measured from property line to retaining wall.
- Side yard setback, measured at future NW Ruth Haines Road and NW Cooperation Way are 5 feet. A boundary line adjustment (BLA) combining the subject and bus parking property north, and completion of the NE Vetter Road vacation of right-of-way and dedication of NW Ruth Haines Road are required (See Conditions P6 and E28).

Building height:

- Building height is 31.6 feet measured according to PMC 18.150.050.
- East and west elevations, measured from finished grade to the highest peak, are 35 feet.
- North elevation is 3 segments, measured from finished grade to roof ridge, provide an average 27.71 feet height.
- South elevation is 3 segments, measured from finished grade to roof ridge, provide an average 28.71 feet height. The east segment finished grade is lower by 3 feet which provides a taller average elevation.
- Heights are summed, divided by 4, providing the average building height of 31.6 feet.

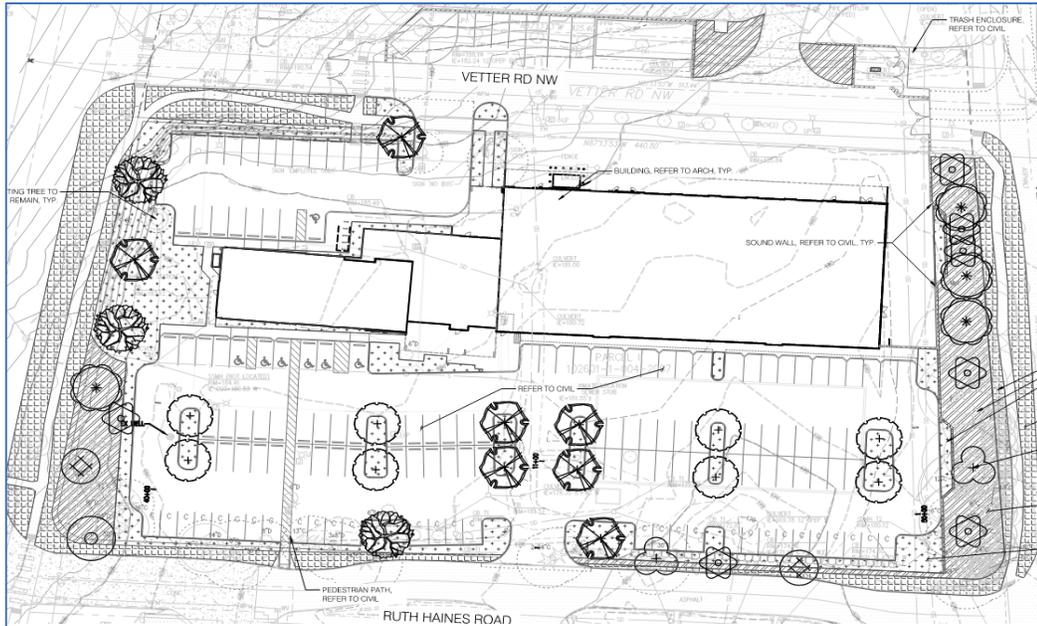
C. PMC 18.90.050 Site planning and design.

1. Landscaping

A minimum of 20% of the property area shall be landscaped. Setback, parking lot, street trees and building perimeter landscaping contribute to the requirement.

Staff Comment. Landscaping covers 20% of the site. Setback, parking lot, street trees and building perimeter landscaping are included. Existing trees along Viking Avenue NW and Vetter Road NE (north-south segment) are proposed to remain(Condition P19).

Preliminary Landscape Plan (Exhibit H)



2. Setback landscaping

- Setback areas are to be landscaped and covered with live plant materials that will ultimately cover 75% of the ground area within three years to include one tree and three shrubs are required for each 300 square feet of area. Certain size and growth requirement for plantings must be met.
- Low impact development stormwater management facilities may be incorporated into setback landscaping when certain requirements are met.
- When adjacent to residential zoning district a combination of sight obscuring fencing, solid screen of evergreen trees and berming is required as approved by the review authority.

Staff Comment. All setbacks are landscaped. Setback area on the site requires 38 trees and 114 shrubs based on the indicated 11,254 square feet setback landscape area (Exhibit H, Landscape Plan). New plantings 5 vine maple, 4 littleleaf linden, 2 zelkova, 1 honey locust, 1 dogwood, 1 Sitka spruce and 1 shore pine will supplement existing evergreen trees along Viking Avenue NW and Vetter Road NE. New and kept trees total more than 38 trees. Shrubs are a mix of Oregon grape, nootka rose, evergreen huckleberry, red-flowering current, oceanspray, whiteleaf manzanita, cluster rose and snowberry. Shrubs may also be planted between future NW Ruth Haines Road right-of-way outside of site security fencing.

Infiltration is identified as infeasible on the site due to shallow depth to intact glacial till.

In addition to trees and shrubs planted along Vetter Road NE, a 10 feet high sight obscuring wall with insets and offsets will be constructed. The wall is designed to match the sound barrier wall constructed along Kitsap Transit property located north.

Sound and Security Wall on Adjacent Transit Property (Source: Google Maps)



Looking west from Vetter Road NE. Single story building is visible beyond.



Looking south from NW Cooperation Way. Tops of busses visible beyond.

3. Street trees

Street trees and related landscaping shall be provided 40 feet on center for arterials and 30 feet on center for collectors within a minimum 5-foot planting strip. Groundcover of sod or other approved groundcover shall be provided.

Staff Comment. Street trees are located along Viking Avenue NW, Vetter Road NE, and future NW Ruth Haines Road. Some existing trees need to be replaced. The applicant requested the City Arborist visit the site during construction, determine which street trees need to be replaced, and the applicant will replace the trees (Condition P19).

4. Parking lot landscaping.

Parking lots with more than ten spaces shall be landscaped. A minimum of 5% of the parking lot area (that area inside parking lot perimeter curbing) shall be landscaped; planting areas shall be a minimum of 5 feet in width. Providing adequate shading opportunities should be taken into account. Parked vehicles may not overhang if the planting area is the minimum width of 5 feet. Wheel stops will be required when any parking space abuts landscaping. Unfenced low impact development stormwater management facilities may be located in parking lot landscaping when feasible and when designed to be integrated appropriately in the landscaped area.

Staff Comment. Over 5% of the parking lot is landscaped. Tree islands, measuring 5 or more feet in width, with under plantings are provided in the south parking lot. Wheel stops are shown on Exhibit D.

5. Building perimeter landscaping.

For any building wall that exceeds an average of 30 feet in height and combined with an unmodulated wall exceeding 100 feet in length (not including loading areas), a planting bed is required, with a hierarchy of plantings for at least 60% of the wall's length.

Staff Comment. This requirement is not applicable. No building wall exceeds 30 feet in height and is unmodulated for a length of 100 or more feet.

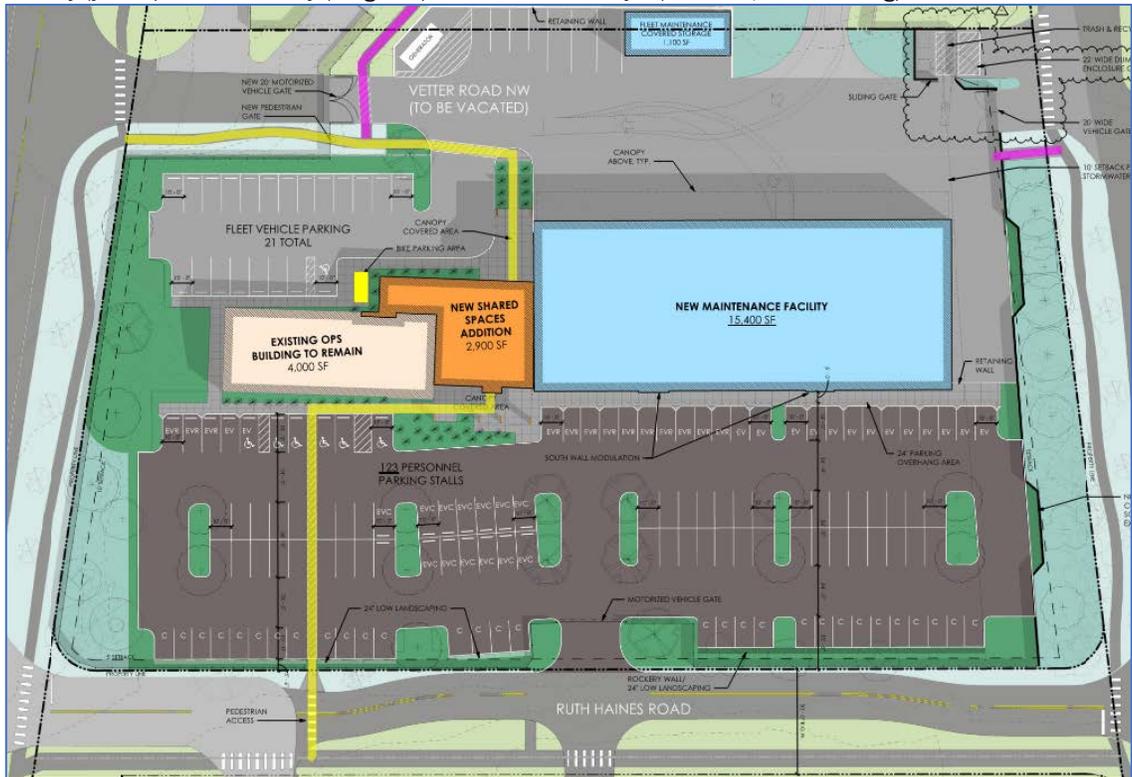
6. On-site pedestrian circulation.

Primary walkways connecting buildings to a fronting street and secondary walkways connecting site buildings are required.

Staff Comment. The site has two primary access walkways. One crosses NW Ruth Haines Road NW connecting the new building south façade to the Park and Ride site south. The second connects the north building façade to NW Viking Avenue along the bus and maintenance access northwest of the building. Secondary access walkway includes a third site access walkway connecting the site to Vetter Road NE and internal access to the north bus parking area and fuel and wash buildings.

The three site access are through locked gates in site perimeter fencing and sound wall.

Primary (yellow) and Secondary (magenta) Pedestrian Pathways. (Exhibit C, CUP Drawing)



7. Building design standards.

Building design is reviewed to facilitate attractive architectural design and scale by avoiding large blank walls, bright colors and providing roof line treatment. Building design shall reflect a Northwest Lodge and/or craftsman style through architectural elements, street scape features, landscaping, artwork, lighting and signage.

Staff Comment. The structure central segment is designed with components reflecting modern lodge including large cover canopy at entrances and clearstory windows. Building design is also consistent with existing North Base structures on properties north and south of the project site. Gable roof will be consistent across the structure. Clearstory windows break the roof plane at the center portion of the structure.

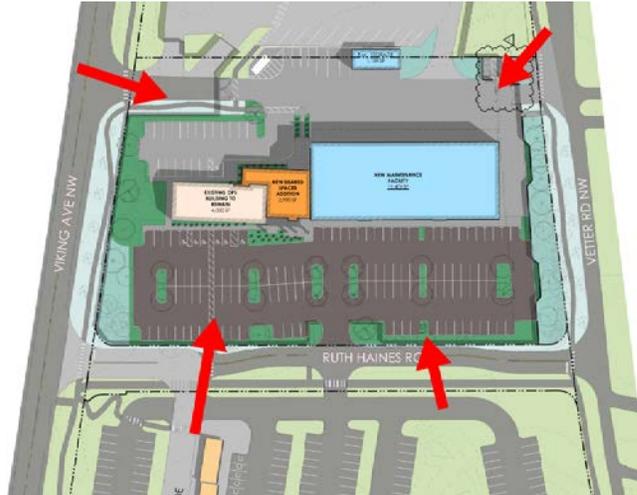
a. Building Facades.

1. Architectural interest is required for all building facades visible from public streets and other publicly visible areas. Publicly visible building walls shall incorporate insets, offsets, canopies, colonnades, wing walls, trellises, building façade landscaping, material variation, multi-planned roof line, or other features which diminish large blank walls.

Building facades visible from public streets and other publicly visible areas that exceed fifty feet in width shall include articulation features to reduce the perceived scale of the building and add visual interest. Multiple articulation methods shall be used, including window patterns, offsets, recesses, staggered walls, stepped walls, pitched or stepped rooflines, overhangs and other elements of the building’s mass. Simply changing materials or color is not sufficient to accomplish this.

Staff Comment. The site will be visible from the west property line along Viking Avenue NW, from the south property line along NW Ruth Haines Road and the Park and Ride facility, and from the northeast corner at Vetter Road NE. Architectural interest is provided through canopy installations, large, covered entryways, wall articulation reducing large expanses, insets and offsets at breaks at building segments, and window and door placements. Stepped roofline between office, operations (driver/staff) area and maintenance structure segments, vertical wall articulation on the south maintenance façade, overhangs at office entrance and maintenance vehicle doors and entrance canopies at commons area.

Public Views of Site



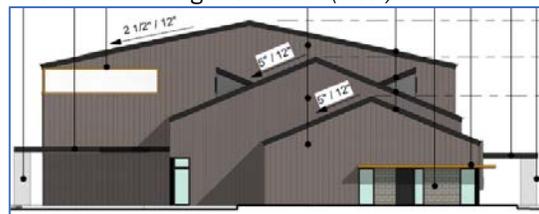
Views from Viking Avenue NW (upper left arrow), NW Ruth Haines Road and Park and Ride (bottom arrows), and Vetter Road NE (upper right arrow). (Exhibit C, CUP Drawing)

2. Buildings shall include design techniques which clearly define the building’s top, middle, and bottom. The following techniques are suggested methods of achieving vertical articulation:
 - Top: sloped roofs, strong eave lines, cornice treatments, horizontal trellises, or sunshades, etc.
 - Middle: windows, balconies, material changes, railings, and similar treatments that unify the building design.
 - Bottom: pedestrian-oriented storefronts, pedestrian-scale building details, awnings, arcades, “earth” materials such as concrete, stone, stucco, etc.

Where appropriate, horizontal elements shall be coordinated (i.e., cornices, window lines, arcades, etc.) in a pattern and height to reflect similar elements on neighboring buildings.

Staff Comment. Sloped roof caps the three distinct building segments. The center section is enhanced with shed dormers and windows facing north and south. The east section upper wall includes clearstory window at the upper roofline and wrapping most of the building segment. Canopies provide shelter at office doors, commons area and over vehicle and person access at the maintenance segment.

Exterior from Viking Avenue NW (west)



West façade shows slope roof of three distinct building segments. Shed roof dormers are visible on the center (operations) segment of the building, (Exhibit E, Design Review)

Vehicle doors are further enhanced with upper windows providing natural light to the interior and solid lower panels. Maintenance segment person doors are solid with sidelight, transom window and covering awning.

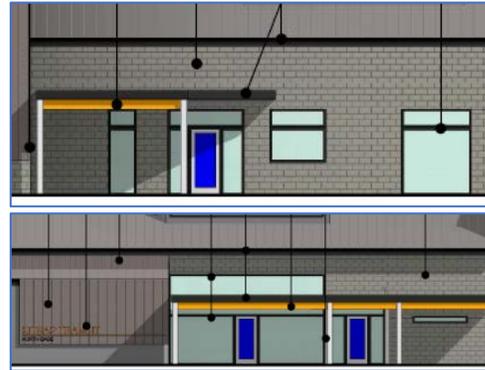
Exterior from NE Vetter Road (north)



Portion of north façade shows vehicle doors, person doors and covering awnings. (Exhibit E, Design Review)

Metal framed glass shopfront doors at the north and south entrances to the center operations building segment are protected by large awnings. The north facing awning covers a wide walkway and the south facing awning includes outdoor space for staff on either side of the main door. Large wall and transom windows accompany the shopfront doors. Smaller person doors on the office/operations building include a vertical light. Posts at north and south office/operations entrances area cast in place concrete similar to the park and ride bus structure south.

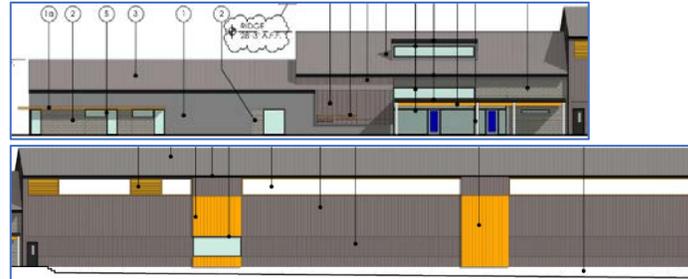
Primary Entrance Doors from NE Vetter Road (north) and NW Ruth Haines Road (south)



Main entrance doors. Top: North facing. Bottom: South facing. (Exhibit E, Design Review)

Other building elements such as person doors, awnings, clearstory windows and materials are consistent with fuel and wash buildings north.

Exterior from NW Ruth Haines Road (south)



Top: Operations and office (west). Bottom: Maintenance (east) (Exhibit E, Design Review)

Signage is consistent across the transit properties.

3. Provide visual terminus to tops of buildings. To avoid a truncated appearance, all structures shall have a visual “cap.” Options include extended eaves; steep pitch hip, gable or saltbox roof form; false pitch roof with appearance of hip gable or saltbox; or projecting cornice of appropriate scale to the building and part of building’s trim detail.

Staff Comment. A wide trim, fascia and gutter provide a roofline edge with slope roof above visually caps the structure.

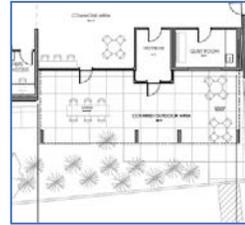
4. Primary building entrances shall be physically oriented to the street and primary pedestrian walkway. The primary entrances to structures, including all entrances to individual tenant spaces, shall be clearly identifiable through architectural design. Specific treatments include, but are not limited to, wall modulation, gables, window clusters, landscape treatment, material/color/texture change, awnings, moldings, planters, and pedestrian amenities, such as benches and tables.

Staff Comment. Primary building entrance for visitors and staff faces south toward NW Ruth Haines Road. The entrance includes a canopy covered outdoor area with supporting pillars, storefront doors in a projecting glass enclosed vestibule with metal framing and corner posts, and gathering spaces with tables and chairs on both sides of the primary door. A fascia under the covering canopy is in accent color.

Primary Entrance Facing South



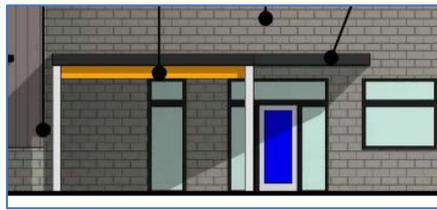
Entrance canopy with accent band under canopy.



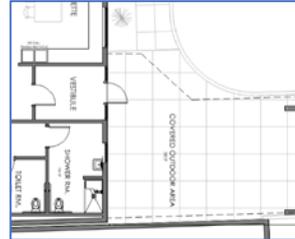
Pillars, entrance vestibule, and outdoor gathering space.

A second building entrance faces the bus parking property which provides pedestrian connection to Viking Avenue NW and access to the bus parking property. The entrance includes a canopy covered outdoor area with supporting pillars, storefront doors in an enclosed vestibule with metal framing and a fascia under the covering canopy is in accent color.

Building Entrance Facing North



Entrance canopy with accent band under canopy



Pillars, covered walkway and entrance vestibule.

b. Materials.

Facades visible from public streets or other publicly visible areas shall provide visual interest by providing a variety of building materials, windows, artwork, or other techniques. Desired materials include brick, wood, horizontal lap siding made of wood or cement-like materials; split-faced block or ground-faced block.

Staff Comment. The building includes vertical and horizontal application of flush panel and corrugated metal siding, standing seam metal roof, concrete and concrete masonry, white/translucent polycarbonate and glass windows, and glass windows and doors. Metal trim surrounds windows and doors. Prefinished sheet metal trim, fascia, and gutter are also identified. (Exhibit E, Design Review).

Materials (Exhibit E, Design Review)



Metal siding applied vertically and horizontally.



Standing seam metal roof.

Continued: Materials (Exhibit E, Design Review)



Concrete and concrete masonry.



Accent color.



Aluminum storefront windows and doors.

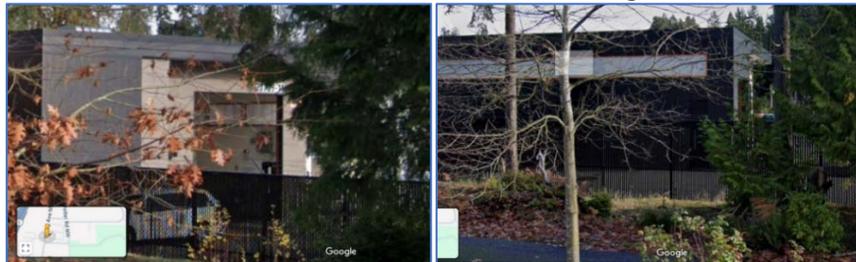
c. Color.

1. Main color of exterior walls is limited to subtle earth tone colors. Soft white, sands, grays, muted pastels, and deep, rich earth colors (terra cotta, forest green) are acceptable.
2. Trim color may be lighter or darker shades of the main color, soft white, or contrast or complement the main color but shall not be bright or bold.
3. Accents or graphics may be brighter than main or trim color and shall be limited to fifteen percent of the facade area, excluding glass. Bright, high contrast color banding is limited to maximum four inches in width.

Staff Comment. Building walls are deep earth black/charcoal metal. Trim color is natural gray concrete along the base and between vehicle doors of maintenance segment, canopy support and wall of operations segment, and between door and windows of office segment. Accent colors are orange and black. Building door and frames, fascia and gutters are black. Canopy accents over person doors, lower vehicle doors, clearstory window frames and south façade building wall offsets and accents are orange. Less than 15 percent of the building is a bright accent color.

Kitsap Transit fuel and wash buildings on property north and Park and Ride shelter on property south utilize the same color and material blocking providing cohesive design.

Kitsap Transit Structures on Adjacent Properties (Source: Google Maps)



Above: Fuel and wash buildings on adjacent property north.



Left: Transit Park and Ride shelter on adjacent property south.

D. PMC 18.90.060 Performance standards.

Compliance with minimum performance standards is required. Certain performance standards are applicable to the Kitsap Transit proposal.

1. Outdoor Storage. Outdoor storage of materials and supplies shall be completely screened with a combination of fencing and landscaping, from adjacent properties and public right-of-way, and be located in the interior of the lot to the extent possible as determined by the planning director.

Staff Comment. Outdoor storage is in a structure with a shed roof approximately 75 feet north of and across the driving aisle from vehicle doors on the maintenance building segment. At its highest the shed roof is 22 feet tall. The location is shielded from public views as it is set into the site with a low surrounding wall and is approximately 230 feet from Viking Avenue NW and 140 feet from Vetter Road NE. Sight obscuring security fencing and sound wall further shields the storage building from public view. The storage structure is designed to match the overall site design and color scheme as shown below. Review of storage structure by the Fire Department and PED Department is required. (Condition 35).

Storage Structure Elevation and Design



Storage structure west and south facades.
(Exhibit E, Design Review)

2. Emissions. Any air emissions shall meet applicable regulations of the Puget Sound Clean Air Agency, and no visible, frequent smoke, dust, or gases shall be emitted.

Staff Comment. The maintenance facility purpose is to keep transit vehicles running at optimum, reducing emissions. The site is designed to maintain electric and multideck buses.

3. Exterior Lighting. Exterior lighting, except for warning or emergency lighting, shall be hooded or shielded so illumination is directed downward and shall be confined to the property boundaries of the light source. Consistency with lighting standards is required.

Staff Comment. All exterior lighting is required to be recessed when in exterior canopies or hooded directing illumination downward when on building or pole. A site lighting and photometric plan is provided with the application (Exhibit G, Lighting Plan)(Condition P9).

4. Noise. Noise levels shall not exceed the maximum allowed in Chapter 173-60 WAC for Class B (commercial) or Class C (industrial) environmental designations as appropriate to the use, or as set forth in the International Building Code requirements.

Staff Comment. A 10 feet tall noise barrier wall will be extended along the property which connects to and matches the sound barrier wall on the site to the north. Project narrative indicates:

- overhead doors may be open, particularly during warmer months and the
- new extended portion of the sound wall will include a vehicle gate, person gate, and garbage/recycle enclosure access gate.
- gates will be constructed of steel framing and skinned with a solid panelized material which will minimize gaps where sound might leak through.

WAC and IBC noise requirements are standard for all uses on the site. Maximum noise levels are set in Washington Administrative Code and International Building Code.

The PED Director supports providing a 10 feet tall noise barrier wall in excess of building code allowed wall height and allowance is appropriate for this use as authorized under conditional use permit (Condition S2). Security wire may be added to top of wall.

5. Exterior Mechanical Equipment. Ground-mounted mechanical equipment shall be screened by landscaping or a decorative wall that incorporates at least one of the materials and colors of the primary structure. The wall shall be of a height equal to or greater than the height of the mechanical equipment being screened. If landscaping is used, the screening material shall be designed to

provide a seventy-five percent opacity one year after planting along the full required height and length of the screening buffer.

Staff Comment. A site generator is located approximately 100 feet north of the building operations segment. The generator is set into the site with a low surrounding wall, and is approximately 120 feet from Viking Avenue NW and 280 feet from Vetter Road NE. Sight obscuring security fencing contributes to screening the generator from public view. Dimensions of the generator are not provided to determine if sight obscuring security fence and site landscaping will screen the generator. Specifications and details for screening of mechanical equipment, including generator, are required with grading permit. (Condition P8).

- 6. Odors. The emission of objectionable odors or matter in such quantities as to be readily detectable at any point beyond the property line of the use causing such odors is prohibited.

Staff Comment. The applicant does not anticipate emission of odors from operations on the site, and if they occur the applicant commits they shall meet applicable regulations of the Puget Sound Clean Air Authority. (Exhibit B, Project Narrative).

- 7. Heat and Glare. Except for exterior lighting, operations producing heat and glare shall be conducted within an enclosed building.

Staff Comment. Welding is identified as an infrequent occurrence and is identified to be conducted in the maintenance building and with use of a portable fume extractor. (Condition P34).

- 8. Trash dumpsters shall be screened from view with a combination of one hundred percent sight-obscuring fencing and vegetative screening. Zoning and construction standards must be met.

Staff Comment. Solid waste enclosure is in the northeast corner of the project. Placement and design are determined accessible by City Public Works staff for collection vehicles. Enclosure will be surrounded by the sound wall, with two access gates.

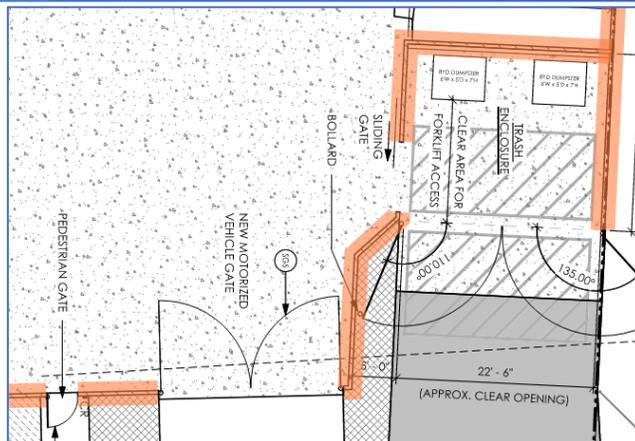
Visible from Vetter Road NE an external double gate “will be constructed of steel framing and skinned with a solid panelized material which will minimize gaps where sound might leak through ... the exterior side of the gates will be complementary to the wall using a finish material that will simulate natural wood or a similar color and aesthetic”. (Exhibit B, Project Narrative). All external gates facing Vetter Road NE will be same appearance and construction. (Condition S2).

The second enclosure gate is internal to the project site for use by transit staff to access dumpsters.

Gated Access at Vetter Road NE



Right gate accesses solid waste. Center gate is emergency access. Left gate is person access. Solid panelized material matching the sound wall is proposed. (Exhibit E, Design Review)



Dumpster enclosure plan. Sound barrier wall highlighted orange. (Exhibit D, Site Drawing)

9. Ground and Soil Contamination. Materials shall be handled in such a manner to prevent ground or soil pollution, or which may contaminate aquifer or other natural drainage systems as required by state and local health agencies.

Staff Comment. Lubricant storage will be internal to the maintenance segment of the new building. Applicant indicates “All above ground storage tanks will be double-walled to provide integrated secondary containment and where required by code, steel grating covered sump(s) will be provided below all storage drums. Containment sumps will be sized for 110% capacity of stored and used drums.” In addition, the applicant indicates the storage structure “will be utilized for Facilities and Marine Maintenance storage materials that do not require a temperature-controlled environment” with “Flooring will consist of soil, grease, water-resistant concrete with clear epoxy sealer and slip resistant finish” and “materials stored under the canopy must be secured, therefore the perimeter of the building will need to be enclosed and lockable.” (Exhibit B, Project Narrative). Proper handling, use and storage of materials is required in state law.

10. Fire and Explosive Hazards. The manufacture, use, processing or storage of flammable liquids or materials, liquids or gases that produce flammable or explosive vapors or gases shall be permitted in accordance with the regulations of the adopted International Fire Code and International Building Code.

Staff Comment. The maintenance and operations building will be sprinkled. The lube room will be separated from adjacent spaces by fire separation as required in building and fire code. Review prior to or with building permit application is required for the storage building. (Condition P35).

Three motorized gates meeting Fire Department requirements will access the site. Staff access at NW Ruth Haines Road; bus and ride share access at Viking Avenue NW; and emergency access at Vetter Road NE. Fire hydrants and riser room are provided on site as required by Fire Code. Proper handling, use and storage of materials is required in state law.

11. In addition to the standards identified above, the city may utilize its authority under the State Environmental Policy Act (SEPA), to identify additional mitigations on impacts to the environment, as determined at time of project review.

Staff Comment. The PED Director supports two modifications for development of the maintenance facility.

- **Parking.** The PED Director supports providing parking in excess of 40 percent of code requirement as surface parking. Allowance is appropriate for this use due to retention of existing structure and need for working and spill containment wells under the maintenance segment of the building which limits the area available for underbuilding parking and makes underbuilding parking unviable and to reduce vehicle noise associated with parking structures as authorized under conditional use permit. (Condition S3). A parking structure may be proposed in future.

Other uses not specifically listed in the parking section shall furnish parking as required by the PED Director based upon the most analogous use. The most analogous use provides 54 vehicle parking spaces are required.

The applicant proposes 150 vehicle spaces and has provided a parking estimate indicating current staffing and future staffing anticipated is in excess of 100 people. Three parking areas are provided on site: staff parking south of the building with 122 spaces; vehicle drop-off for maintenance north of the building with 21 spaces; and a maintenance vehicle parking area with 7 spaces.

See below PMC 18.90.090 Parking and loading standards for more information.

- **Security and Noise Barrier Wall and Fencing Height.** The PED Director supports provision of a 10 feet tall noise barrier wall in excess of building code allowed wall height. Allowance is appropriate for this use to reduce potential noise to the residential properties located east of the project site as authorized under conditional use permit. (Condition S2). Security wire may be added to top of wall. A building permit is required for the wall. See above PMC 18.90.060 Performance standards item 4 for more information.

No additional impacts to the environment are identified with project review.

E. PMC 18.90.090 Parking and loading standards.

Parking and loading standards are found in this section and in PMC 18.140.

Staff Comment. Uses not specifically listed shall furnish parking as required by the director based on the most analogous use. A total of 54 vehicle spaces are required as calculated when considering most analogous use.

- *Parking calculation:*
 - 15,400 sf maintenance facility calculated at 1 space per 500 sf
 - 4,033 sf existing building and 2,993 sf shared spaces addition calculated at office / professional services at 1 space per 300 square feet.

$$30.8 + 23.4 = 54 \text{ spaces required}$$

- *Proposal:* A total of 150 spaces are shown on the site plan. Application documents identify city parking requirement and does not meet parking needed at the site. Conclusion in Exhibit B, Project Narrative and Design Review Narrative indicates parking needed on site are 60 existing staff and 44 new staff for a total of 104 anticipated staff.

A 122 space south parking lot accommodates staff parking. Driver shift changes necessitate parking accommodate those on service and readying to be on service. Overlap in shift parking is typical of bus services sites. The site also serves as operations and dispatch and a satellite work location for office staff. Visitors to the site are infrequent.

Twenty-one parking spaces north of the operations building provide a location for transits fleet vehicles such as carpool vehicles needing maintenance.

Seven spaces are located north of the maintenance bays for service vehicles.

The director supports providing surface parking in excess of code requirement as surface parking. Underbuilding or parking structure is not required for parking provided in excess of forty percent over the minimum required. (PMC 18.140.030 B). Retaining existing structure and need for working and spill containment wells under the maintenance segment of the building limits the area available and makes underbuilding parking unviable. A surface parking allowance for this use is appropriate and is authorized through the conditional use permit.

Deliveries to the site are by Kitsap Transit vehicles. An occasional delivery may schedule through another carrier. Delivery vehicles will enter and leave through the Viking Avenue NW gate. (Condition P36). Loading area is north of the maintenance segment of the building. No designated loading dock is located on the site. Parking or staging of delivery trucks on public roadways is prohibited. (Condition P36).

Staff access to the site will be NW Ruth Haines Road. Viking Avenue NW is the primary access for transit vehicles entering and exiting the site.

Off-street parking and loading is reviewed under PMC 18.140 Off-street Parking and Loading as indicated below. A parking plan is shown on site plan drawings. (Exhibit C).

- Parking spaces and accessible routes are required to be consistent with IBC and state ADA requirements. Seven ADA stalls are provided on site. One space is in the north parking lot. Six spaces, including one van and one EV space, are provided in the south parking lot.
- Internal parking drive aisles are designed as required and parking stall dimensions meet the requirement.
- All parking is located on the same property as the maintenance and operations building.
- Forty-three spaces are sized for compact vehicles including 13 designated for EV parking. All are located in the south parking lot. Twenty eight percent of site parking is designated compact.
- Where landscaping provides a break in the group of spaces, the landscape island shall extend at least one foot into the circulation aisleway to provide a visual narrowing of the aisleway. Drive aisle will be 22 feet wide between landscape islands as shown below. (Condition P37). See markup below.

Parking Lot Islands



South parking lot landscape islands required to extend one foot into the drive aisle are highlighted in yellow. (Exhibit C, CUP Drawing)

- Wheel stops are required as shown on Exhibit D, dated February 2025 (Revision 2). Additionally, the front 2 feet of parking stalls closest to NW Ruth Haines Road is low landscaping and the front 2 feet of parking stalls closest to the maintenance building are concrete sidewalk.
- Parking lot lighting is directed downward as required. See section 18.90.060 item 3 above.

Parking Lot Lighting

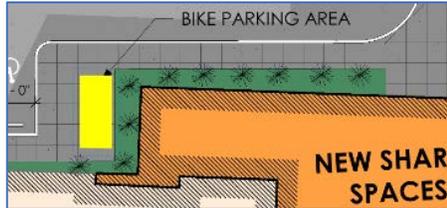


Left: Exterior wall mount lighting. Right: Typical 20 feet tall light pole. (Exhibit G, Lighting Plan)

- Application project narrative identifies the north parking lot is for use of rideshare vehicles.
- Bicycle parking for 10 bicycles is required. Stationary racks that support the bicycle with at least one point to which the user can lock the bicycle and both wheels and frame (with removal of the front wheel) with a high security U-shaped lock or cable lock are required. (Condition P14).
 - Bicycle parking is required at a ration of one space per twenty vehicle spaces plus 2 additional bicycle parking spaces.

$$[(154/20) + 2] = 7.7 + 2 \text{ rounded to } 10 \text{ spaces required.}$$
- Bicycle parking is provided north of the operations building segment. Bicycles are to be accommodated exclusive of walking routes. (Condition P14).

Bicycle Parking



Left: Bicycle parking at north parking lot is highlighted yellow. (Exhibit C, CUP Drawing) Right: Bicycle parking design. (Exhibit E, Design Review)

- Buildings and accessory structures shall be provided with EV charging stations, EV-ready parking spaces, and EV-capable parking spaces in accordance with IBC Section 429, as amended. Calculations shall be consistent with Section 18.150.020. (Condition P38)
- Where charging equipment is provided within an adjacent pedestrian circulation area, such as a sidewalk or accessible route to the building entrance, the charging equipment shall be located so as not to interfere with accessibility requirements of WAC 51-50-005. (Condition P39)
- Each charging station space shall be posted with signage indicating the space is for electric vehicle charging purposes. Days and hours of operations may be included if time limits or tow away provisions are to be enforced. (Condition P40).

EV Parking Stall Signage



Example signs for EV parking stalls. (PMC 18.140.080)

F. PMC 18.90.100 Signage standards

Signage standards of this section and Chapter 18.170 apply.

Staff Comment. A wall-mounted sign is shown on the south building façade. Lettering design matches signage on adjacent transit properties north and south. Signage requires a building permit. (Condition P13).

Building Signage



Left: Wall mounted sign at south building entrance. Right: Lettering style on transit's nearby buildings. (Exhibit E, Design Review)

A freestanding sign may be located at the NW Ruth Haines Road entrance. External sign lighting is allowed. Signage consistency with the North Base and Park and Ride freestanding signs at Viking Avenue NW is required. Signage requires a building permit. (Condition P13).

Freestanding Sign



Left: Freestanding sign at Viking Avenue NW entrance to transit's north property. Right: Freestanding sign as Viking Avenue NW entrance to transit's south property. (Source: Google Maps)

V. **PMC 18.230 CONDITIONAL USE PERMITS**

A conditional use permit is a mechanism by which the city may require special conditions on development or on the use of land in order to ensure that designated uses or activities are compatible with other uses in the same land use district and in the vicinity of the subject property.

In permitting a conditional use, the review authority may impose, in addition to regulations and standards expressly specified in this title, other conditions found necessary to protect the health, safety and welfare of the surrounding property(ies), neighborhood, and the city as a whole.

A. PMC 18.230.060 Decision Criteria

The review authority may approve, or approve with conditions, the application for a conditional use permit, if it meets the following criteria. Applications that do not meet all decision criteria will be denied.

1. The conditional use will not be materially detrimental to uses or property in the immediate vicinity of the subject property; and
2. The design is compatible with and responds to the existing or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity; and
3. The conditional use will be served by adequate public facilities including streets, fire protection and utilities; and
4. The conditional use complies with all applicable requirements of this code.

Staff Comment.

- With conditions imposed, the new North Base Maintenance Facility will not be detrimental to uses or property in the immediate vicinity of the subject proposal.
- The design is compatible with and responds to the existing character of the LI zone and adjacent RL zone.
- The property is, and will continue to be, serviced by all public utilities upon completion of the project.
- As shown in this staff report, the use of the subject property for a transit maintenance facility complies with all applicable requirements of the Zoning Code, subject to conditions of approval, including deviations for surface parking lot and security and sound wall.

B. PMC 18.230.070 Limitations of permit

A CUP shall become void two years after approval, or after such greater or lesser time as may be specified as a condition of approval, unless within that time the required building construction, alteration or enlargement has been commenced and diligently pursued, or if no such construction, alteration or enlargement is required, that the permitted use is regularly conducted on the premises. The planning director may extend the permit for one one-year extension.

Staff Comment. The conditional use permit will expire five (5) years after approval and a one (1) year permit extension may be granted by the PED Director. Expiration and extension are consistent with site plan review permit approval. (Condition P4).

VI. PMC 18.270 SITE PLAN REVIEW

All new developments and modifications to existing developments shall require site plan review and approval prior to the issuance of any building permits, establishment of any new uses, or commencement of any site work.

A. PMC 18.270.050 Approval criteria

1. Compliance with Applicable Standards. The proposed development shall comply with all applicable design and development standards contained in this title and other applicable regulations.
2. Adequacy of Public Facilities. The applicant shall demonstrate availability of adequate public services, e.g., roads, sanitary and storm sewer and water, available to serve the site at the time development is to occur, unless otherwise provided for by the applicable regulations.

Staff Comment.

- The proposed development complies with all applicable design and development regulations as conditioned and through conditional use permit.
- The City Engineering Department has reviewed the proposed project, evaluated the adequacy of utilities and streets, and has found the project, in combination with the Engineering and Public Works Conditions of Approval, to provide adequate public facilities.

B. PMC 18.270.070 Permit limitations

Site plan review approval shall be effective for a period of five years from the date of approval. The site plan review approval shall expire if:

1. Substantial construction of the approved plan has not begun within a five-year period; or
2. Construction on the site is a departure from the approved plan.

Staff Comment. The site plan review permit will expire five (5) years after approval and a one (1) year permit extension may be granted by the planning director. (Condition P5).

VII. STATE ENVIRONMENTAL POLICY ACT (SEPA)

A combined Notice of Application (NOA) and Optional DNS was issued on September 5, 2024 (Exhibit P). Commented SEPA environmental checklist was circulated September 5, 2024. (Exhibit Q). The public comment period ended September 19, 2024, at 4:30 pm. No public comment was received. The SEPA Threshold Determination was issued April 11, 2025, using the Optional process (WAC 197-11-355), an additional comment period is not required (Exhibit R).

VIII. TITLE 19 PROJECT PERMIT PROCEDURES

Review Step	Date
Pre-Application Conference	January 9, 2024
Neighborhood Meeting Held	June 13, 2024
Pre-Application Waiver	August 13, 2024
Counter Complete	August 23, 2024
Technically Complete	August 26, 2024
Notice of Application w/ Optional DNS	September 5, 2024
Request for Revision and Additional Information Sent	September 19, 2024
Revisions Received	December 5, 2024
Request for Revision and Additional Information Sent	January 6, 2025
Revision Received	February 13, 2025
SEPA Threshold Determination Issued	April 11, 2025
Public Meeting Notice Issued	April 15, 2025
Staff Report Issued	April 15, 2025
Planning Commission Public Meeting	April 22, 2025
Public Hearing Notice Issued	May 8, 2025*
Hearing Examiner Public Hearing	May 22, 2025*
* Subject to change	

IX. STAFF COMMENT AND RECOMMENDATIONS

Comments: This project as proposed is consistent with the Poulsbo Comprehensive Plan and Zoning Ordinance.

Recommendation: Staff respectfully recommends approval of the Kitsap Transit North Base Maintenance Facility Conditional Use Permit and Site Plan Review, Planning File P-07-24-24-02 as presented and subject to all Conditions of Approval contained herein.

X. EXHIBITS

- A. Application
- B. Project and Design Review Narrative
- C. CUP Drawing
- D. Site Drawing (Civil Drawing)
- E. Design Review
- F. Oblique Views
- G. Lighting Plan
- H. Landscaping Plan
- I. Noise Barrier and Security Wall
- J. Critical Area Assessment
- K. Storm Drainage Report
- L. Geotechnical Report
- M. Hydrogeologic Assessment
- N. Concurrency Application
- O. Parking and Trip Generation Analysis
- P. NOA with optional DNS
- Q. SEPA Environmental Checklist – commented
- R. SEPA Threshold Determination
- S. Engineering Site Plan Review Memo with Attachments

KITSAP TRANSIT NORTH BASE MAINTENANCE FACILITY CUP AND SPR CONDITIONS OF APPROVAL PLANNING FILE P-07-24-24-02

Following are the Planning and Economic Development Departments Conditions of Approval:

General

- P1. Development of the site shall be in conformance with the site plan, elevations, and associated documents identified in Exhibits C-I, stamped approved in Planning File No. P-07-24-24-02 and subject to the conditions of approval contained herein.
- P2. Approval of this site plan subject to the attached conditions and mitigations does not vest or limit the project to those conditions and mitigations until the time of building permit application. If during the time between site plan approval and building permit application, there are changes to applicable regulations such conditions may apply to this project.
- P3. Development of the site shall also be in conformance with the reports and peer review documents identified in Exhibits J-O.
- P4. Conditional Use Permit shall become void 5 years after approval, unless within that time the required building construction, alteration or enlargement has been commenced and diligently pursued. The planning director may extend the permit for one one-year extension. The planning director may extend the Conditional Use Permit and site plan review permit for one one-year extension as outlined in PMC 19.90.030.
- P5. Site Plan Approval is effective for a period of 5 years from the date of approval. The site plan approval shall expire if substantial construction of the approved plan has not begun within the 5-year period. Site Plan approval will become void if construction on the site is a departure of the approved plan. An extension not to exceed one year may be granted by the Director if applied for 30 days prior to the expiration date and found to meet the criteria provided in PMC 18.270.080.B.(1-4).
- P6. The site includes a portion of the Kitsap Transit owned property north of NW Vetter Road. A boundary line adjustment (BLA) review and recording for the purpose of lot aggregation must be completed before Building Permit issuance.
- P7. Building height verification shall be submitted with the building permit drawings and additional review will ensure that the construction of the buildings shall remain within the required height limit. Building height verification shall be required via the "Average Building Height" form submitted with the building permit application(s).
- P8. All HVAC equipment, pumps, heaters, and other mechanical devices shall be screened from view from adjacent streets and property. Ground-mounted mechanical equipment shall be screened by landscaping or a decorative wall that incorporates at least one of the materials and colors of the primary structure. The screening shall be shown on the building permit submittals. Specification and details for screening of mechanical equipment, including generator, shall be provided at the time of grading permit.
- P9. All exterior lighting shall be pointed downward and shielded from direct observation from the air, adjacent properties, and public rights-of-way. Lighting under canopies must be recessed. Lighting shall meet all standards identified in the PMC. The applicant shall include a final lighting/photometric plan, including specification of fixtures, at the time of Tree Cutting and Clearing (TCC) permit submittal.
- P10. The trash receptacles(s) shall be screened from public view on at least 3 sides by opaque sound wall 10 feet in height and on the fourth side by an opaque gate matching sound wall design height. Gated access to the site may be installed. Dumpster enclosures shall be constructed out of the same materials as the sound wall. Trash receptacles shall be reviewed at the time of TCC permit submittal.
- P11. Modifications of an approved site plan shall be processed pursuant to Title 19 regarding post-decision review.
- P12. Prior to issuance of any permits related to the construction of retaining walls (if applicable), appropriate property rights, including temporary construction easements and permanent wall maintenance easements on impacted adjacent properties, must be obtained authorizing the construction of the boundary walls.
- P13. Separate sign permits are required for any proposed signage and will be reviewed for compliance with the City's signage standards PMC 18.80.120 and 18.170.
- P14. The manufacturer specification for the bicycle rack(s) shall be provided with the building permit submittal. Certificate of Occupancy will not be issued until the bicycle parking racks have been installed. Bicycle parking for 10 bicycles is required. Stationary racks that support the bicycle with at least one point to which the user

can lock the bicycle and both wheels and frame (with removal of the front wheel) with a high security U-shaped lock or cable lock are required.

- P15. While there are no known archaeological resources on this site, in the event archaeological artifacts are uncovered during construction, activity shall be halted immediately, and the State Historic Preservation Office and Tribes will be contacted.

Tree Cutting and Clearing (TCC) Permit

- P16. A TCC permit application shall be submitted and reviewed concurrently with the Grading application. The application form and associated fees are available on the PED Department website.
- P17. A detailed site phasing plan (if applicable) shall be submitted with the TCC permit. The phasing plan shall identify site improvements to be completed in each phase, including parking, pedestrian pathways, landscaping, and recreational amenities.
- P18. The TCC and grading permits shall be “closed-out” prior to a request for Certificate of Occupancy.

Landscaping

- P19. At the applicant's request, street trees needing replacement will be identified during construction by the City Arborist. Trees identified by the City Arborist requiring replacement will be. The City Arborist will determine species of replacement trees. All review work and plants are at the expense of the applicant. Installed landscaping will be included in a landscape bond required for the project. Root barriers shall be used to protect sidewalks and roadways from root heave.
- P20. All landscaping shall be accompanied by a suitable irrigation system designed by a licensed landscape contractor or landscape architect. An irrigation plan is required to be submitted with the final landscape plan at the time of TCC permit submittal. Automated systems should include moisture sensing automatic shut-off controls. Landscape irrigation systems require a separate building permit.
- P21. Maintenance bonding for all landscaping shall be required prior to close-out of the TCC permit. Estimates or bid for landscaping work and materials, including irrigation, shall be provided. Estimates shall match the “as-built” drawing and identify plant name, common name, size at planting, and number each that was planted. Performance bonding for installation of landscaping (except for street trees) will not be considered.
- P22. A maintenance assurance device, agreement or bond for two years will be required to ensure the existing vegetation remains healthy and additional vegetation appropriate to the overall site's landscape plan must be installed if the existing vegetation does not survive.
- P23. Landscaping materials shall be those which best serve the intended function and shall be appropriate for the soil and other environmental conditions of the site. Drought-tolerant, low water plant materials shall be encouraged.
- P24. Proposed freestanding and/or monument sign(s) shall be shown on the Final Landscape Plan. A separate building permit is required for construction of the sign(s).
- P25. A final “as-built” drawing landscaping plan and irrigation plan shall be provided to the city prior to close-out of the TCC permit.
- P26. Trees to be kept on site and tree protection fencing shall be shown on tree cutting and clearing plans, construction civil plans and landscape plans.
- P27. Tree Protective Fencing.
- Details on tree protection fencing shall be included in drawings submitted with the TCC and Grading permits.
 - No site work shall take place on the site until tree protective fencing is installed.
 - Tree marking and installation of protective fencing shall be under supervision of ISA certified project arborist, and inspected by City Arborist, at the cost of the applicant.
 - Tree protection fencing shall be installed 5 feet outside the dripline of trees to be retained.
 - No mechanical equipment or work shall be permitted inside protective fencing.
 - No storage, equipment, or vehicular traffic shall be permitted inside protective fencing.
 - Tree protective fencing shall be shown on all submitted materials including construction and landscape drawings.
 - Protective fencing shall not be moved or removed without consultation with the City Arborist.
- P29. The ISA certified project arborist shall be on site to observe and direct tree protection measures to ensure best arboricultural practices are followed.

- P30. Tree marking and protection fencing shall be under the supervision of project arborist and when completed, the city is notified for inspection by the City Arborist, at the cost of the applicant.
- P31. The project arborist and their contact information shall be identified at the preconstruction meeting.
- P32. A follow-up risk assessment of the proposed areas of trees to be kept on site shall be conducted by the project arborist once the tree cutting, clearing and grading has occurred, and submitted to the city for peer review. If needed, the tree retention numbers shall be amended to reflect any necessary removals and additional plantings, and an arborist report shall be provided to the City prior to close-out of the TCC permit and shall be peer reviewed by the City Arborist. The arborist report shall verify the tree retention plan and project conditions are met. The report shall also identify maintenance recommendations for a period of 5 years or reaffirm maintenance recommendations of prior report. Additional assessment may be required if deemed necessary by the PED Director in consultation with the City Arborist. Reports and reviews are at the cost of the applicant.
- P33. The City shall be notified of any impact or pruning of significant tree roots prior to close-out of the TCC permit. The city may require peer review by the City Arborist, at the cost of the applicant.

Other

- P34. Except for exterior lighting, operations producing heat and glare shall be conducted within an enclosed building.
- P35. Storage building shall be reviewed no later than building permit for consistency with Fire Department and PED Department requirements. Storage shall be completely screened with a combination of fencing and landscaping from public roadways. Site access motorized gates shall meet Fire Department requirements.
- P36. Delivery vehicles shall not stage on public roads. Delivery vehicles will enter and leave through the Viking Avenue NW gate.
- P37. Where parking lot landscaping provides a break in the group of spaces, the landscape island shall extend at least one foot into the circulation aisleway to provide a visual narrowing of the aisleway.
- P38. Buildings and accessory structures shall be provided with EV charging stations, EV-ready parking spaces, and EV-capable parking spaces in accordance with IBC Section 429, as amended. Calculations shall be consistent with Section [18.150.020](#).
- P39. Where charging equipment is provided within an adjacent pedestrian circulation area, such as a sidewalk or accessible route to the building entrance, the charging equipment shall be located so as not to interfere with accessibility requirements of WAC [51-50-005](#).
- P40. Each charging station space shall be posted with signage indicating the space is for electric vehicle charging purposes. Days and hours of operations may be included if time limits or tow away provisions are to be enforced.

DocuSigned by:

Heather Wright

4/16/2025

Heather Wright, AICP, Planning and Economic Development Director

Date

Following are the Engineering Department's Conditions of Approval:General

E1. All water, wastewater, and stormwater facilities and streets shall be designed by a professional civil engineer licensed in the State of Washington. The applicant is responsible for the design and installation of the facilities. In the event that there is a conflict between standards, the more restrictive standard shall apply as determined by the City Engineer.

E2. Land use permit approval shall not waive any requirements for the applicant to (a) obtain all appropriate permits; (b) pay all required fees and deposits; and (c) provide the City with adequate construction plans for approval which conform to City codes and standards. Any utility plans, details, and drawing notes associated with the approved site plan drawing are approved in concept only and are not considered approved for construction. Approval of the site plan does not constitute approval of any construction drawings submitted with the site plan approval documents. Civil construction drawings must be submitted directly to the Engineering Department. For site plans, it is not acceptable to submit the civil drawings with the building plans to the Building Department. Approved drawings for WSDOT improvements shall be included with the construction drawing package. Construction drawings submitted shall be consistent with the approved WSDOT improvement drawings.

E3. Construction plans for the following shall be reviewed and approved by the Engineering Department and Public Works Department: storm drainage and street improvements (including signage and pavement markings), sanitary sewer, water, and interim and permanent on-site erosion control systems. Prior to final project construction approval the applicant shall: construct the required improvements per City standards, and submit "as-built" drawings on paper, and electronically (compatible with the AutoCAD version utilized by the City at the time of submittal), dedicate easements, convey utility ownership as determined by the City, and post a maintenance bond(s).

E4. All plan review and project inspection and administration expenses shall be paid for at the developer's expense consistent with the fee and deposit schedule adopted by City ordinance in effect at the time of construction. Plan review fees shall apply to the original drawing submittal and one re-submittal. Subsequent submittals will require payment of hourly charges. Fees are non-refundable. Deposits are required for payment of actual expenses incurred by Engineering Department staff for project administration and inspection. If the City Engineer determines that the magnitude or complexity of the project requires full or part-time on-site inspection in addition to the inspection by City staff, he may contract with a duly qualified inspector or hire additional personnel to provide inspection, testing, or other professional services for the City in connection with the construction. Deposits for Engineering Department services or outside professional services shall be paid in advance. The deposits are estimates and may require replenishment. Deposits may be required at the time of, or after, payment of any fees. Unused deposits are refundable.

E5. At any point in the process of application approval, construction plan review, or construction, the City Engineer may hire an independent consultant to review and comment on any, or all, utilities or sitework (for example, storm sewer, sanitary sewer, water, roads/streets, retaining walls, slopes) proposed by the applicant. The applicant shall make a cash deposit which will be used to pay for any independent review required by the City Engineer. If additional funds are required, the applicant shall immediately deposit the requested amount. Any unused funds will be refunded. Acceptance of the proposal and consultant comments shall be at the discretion of the City Engineer.

E6. "City of Poulsbo Construction Standards and Specifications" are published on the City website within the Public Works/Engineering Department page. Unless specified otherwise within Conditions of Approval these standards shall be followed.

E7. The applicant shall adhere to all recommendations of the applicant's geo-technical engineer, biologist, and the City's consultants.

E8. The civil construction drawings shall include plans for: grading, water, sewer, storm, streets, dry utilities, street lighting, signage/stripping, and composite wet utilities. Other plans may be required depending on site-specific conditions. Profiles and details for the wet utilities shall also be provided.

E9. Construction drawings will be rejected, without review, if the following drafting requirements are not met:

- a. Construction plan size shall not exceed 24"x36". The minimum drawing scale shall be 1:40 horizontal and 1:5 vertical. A larger scale may be required for legibility.
- b. Utilities shall be shown on plan/profile sheets. Each sheet shall have the corresponding plan/profiles on the same sheet with aligned stationing.

- c. Labels from the various overlapping AutoCAD layer shall be legible.
- d. All elements on the drawings shall be legible as determined by the City Engineer.

Clearing, Grading, and Erosion Control Refer to Const Stds

E10. A Tree Cutting and Clearing and/or Grading Permit is required prior to any land-disturbing activity on the site (PMC 15.35, 15.40). The permit may include restrictions as to the limits of any particular area or phase that can be cleared and graded at any one time or during any construction season. Additional restrictions may be placed on the permit in regard to seasonal weather conditions. At any time, the City Engineer may restrict activities or access to portions of the site which would be detrimental to maintaining erosion and sediment control.

E11. Application must include the following documentation in digital format.

- Full size rendering of the project construction drawings.
- Final Drainage Report
- Supporting reports (Geotech, Environmental Assessment or as applicable)
- Payment of funds consistent with the Grading Permit Application

E12. The developer's engineer shall submit a completed NPDES Permit Appendix 7 Worksheet along with other required stormwater application documents.

E13. Applicant shall obtain a Construction Stormwater General permit from Department of Ecology prior to project start.

Stormwater

E14. A Final Stormwater Report will be required with Construction Drawing Application The stamping/cover page of this Final Storm Report shall include the following text:

"I hereby state that this Drainage Report has been prepared by me or under my supervision and meets the standard of care and expertise which is usual and customary in this community of professional engineers. The analysis has been prepared utilizing procedures and practices specified by the City of Poulsbo and within the standard accepted practices of the industry. I understand that the City of Poulsbo does not and will not assume liability for the sufficiency, suitability or performance of drainage facilities prepared by me."

E15. The project shall be designed to the 2019 Stormwater Management Manual for Western Washington. A final stormwater report, Stormwater Pollution Prevention Plan (SWPPP) and Temporary Erosion and Sedimentation Control (TESC) plan shall be submitted with the grading permit submittal.

E16. This proposed project is subject to General facility Fee as per PMC 13.70.110.

E17. Ownership and maintenance of stormwater systems located on commercial private property will remain the responsibility of the property owner. Prior to the use of the development or redevelopment project, the owner shall sign and record a maintenance covenant using the City's form (Reference: PMC 13.17).

E18. All temporary and permanent storm system and erosion control measures shall be designed, constructed, maintained, and governed per PMC 13.17.060.

E19. A spill control type oil/water separator shall be installed in the stormwater system at the most downstream point of the site.

E20. An Erosion Control Plan and accommodating SWPPPs will be required to be submitted with Construction/Grading Permit Application which demonstrates compliancy with the Final Stormwater Report and provides sufficient protection against silt-laden runoff from leaving the project.

Water

E21. Ownership of any water main and appurtenances shall be conveyed to the City prior to final construction approval. An easement for access and maintenance of the water main and hydrants within the site shall be legally described and dedicated to the City prior to final construction approval. The easement shall be shown on the construction drawings and as-built drawings. The easement shall be fifteen feet wide and include a ten-foot radius around fire hydrants.

E22. Front footage fees shall apply for connection to the Viking Avenue water main pursuant to PMC 13.70.310. The current fee is \$16 per lineal foot of frontage. The fee is due prior to issuance of clearing/grading permit.

E23. Existing well tag# AAA-563 shall be decommissioned per Kitsap Health District requirement.

All Utilities

E24. Water and Sewer general facility fees are calculated based upon Equivalent Residential Units (ERUs) per PMC 3.12 and 13.70 and are based on water meter size.

E25. EV charging stations conforming with the requirements of the current adopted version of the International Building Code shall be shown on utility drawings.

Streets

E26. Unless otherwise approved by the City Council, street sections & driveways shall conform to adopted City standards. (refer to Developer’s Guide – Section 2 – Street Standards, available online; <http://www.cityofpoulsbo.com/publicworks/ConstructionStandards.htm>)

E27. As a condition of site plan development, the construction of frontage improvements is required. The applicant is responsible for minimum improvements to Viking Avenue. Typical frontage improvements including sidewalk currently exist, but all non-ADA compliant sidewalks must be surveyed to ensure full compliance and brought up to current standard if deficient.

E28. Vacation of Vetter Road as approved by the City with City Council ordinance number 2009-07 shall be completed prior to grading permit issuance. This will include completion and closeout of the Ruth Haines Roadway project and dedication of the associated right of way and utility easements as conditioned in Section 2 of ordinance 2009-07 and recording of the ordinance by the City Clerk.

E29. All intersections, crosswalks at intersections, sidewalks and driveway drops shall meet current ADA standards in place at time of construction (including 28 CFR Part 36). Construction drawings shall include sufficient intersection grade and slope details to determine ADA compliance. Individual curb ramp details for each curb ramp sufficient to show compliance and provide sufficient details for construction shall be provided.

E30. GMA Transportation Impact Fee Ordinance (PMC 3.86) has been approved by City Council. This establishes a transportation impact fee assessment per ADT payable at time of Building permit issuance. The trip generation and justification memo submitted by the applicant along with the concurrency application documenting 136 new ADT has been accepted by the City. As of the date of this memo, traffic impact fees are estimated to be 136ADT x \$564/ADT = \$76,704.

E31. Vehicle restraint and/or guardrails are required at locations where retaining walls create a significant hazard. The location and type shall be approved by the City Engineer prior to construction plan approval.

Other

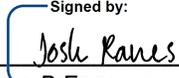
E32. All bonds, conveyances, and easements dedicated to the City shall be on the City's forms.

E33. A spill prevention plan shall be submitted with grading permit submittal materials.

E34. Any agreements made between the applicant and another property owner related to utilities, easements, right-of-ways, or ingress and egress shall not be in conflict with City codes or ordinances. No agreements between the applicant and the property owner shall exempt either party from obtaining proper City approval for land use activities regulated under the Poulsbo Municipal Code.

E35. A Public Property Construction Permit is required when connecting to City-owned utilities or performing other work within the City right-of-way or other public/City-owned property (PMC 12.08). The permittee shall be responsible for repair and/or restoration of any damage to City property (such as sidewalks, curbs, gutters, pavement, and utilities) that occurs as a result of his operations under this permit.

E36. The applicant shall be responsible for obtaining all required easements and rights-of-way. Copies of all recorded easements shall be provided to the City Engineer.

Signed by:


Joshua Raney P.E.,
City Engineer

4/15/2025

Date

Following are the Public Works Department's Conditions of Approval:Service Availability

PW1. The City of Poulsbo has determined that, as of the date of this development approval, the City has sufficient water supply to serve the development. This determination is not, however, a guarantee that sufficient supply will exist at the time of connection to the City's water system is applied for and the City expressly disclaims any such guarantee. The City allows connections to its water system on a first-come, first-served basis and the City may or may not have an adequate supply of water available to serve the development at the time connection is applied for. Pursuant to RCW 19.27.097, verification that an adequate water supply exists to serve the development will be required at the time a building permit is applied for and issuance of a certificate of water availability by the City at the time will be necessary before the ability to connect to the City's water system is assured.

PW2. Sewer conveyance and treatment demand to serve the City's growth is anticipated in the City's Comprehensive Sewer Plan, the Poulsbo sanitary sewer Capital Improvement Plan (CIP) and the Kitsap County Capital Improvement Plan. The City's CIP identifies improvements to serve the projected growth of the City based on historic growth rates, and adequately provides for the development of the 4th Ave Apartment project. This determination is not, however a guarantee that sufficient capacity will exist at the time connection to the City's sewer system is applied for and the City expressly disclaims any such guarantee. The City allows connections to its sewer system on a first-come, first-served basis and the City may or may not have adequate sewer capacity to serve the development at the time connection is applied for. Verification of available sewer capacity will be required prior to issuance of building permits.

Water

PW3. The proposed building shall be connected to city water. Current water service is from an 8" main in Vetter Road. Existing service to existing structure may be upsized to serve both structures, or the new maintenance facility building may be served from a separate meter. In either case, size meter by fixture count submitted to Building Department for review.

PW4. Any existing water service or stub to the site and not utilized must be decommissioned at the main if not used. Show new connection/stub decommissioning on construction drawings.

PW5. New service shall be installed for domestic, fire, and irrigation for the proposed project unless sufficient stub outs already exists and meet current standard for connection as determined by the Public Works Department.

PW6. Service connection to the City water system shall be the responsibility of the property owner and shall comply with state and local design and development standards.

PW7. Pursuant to WAC 246-290-490, the water services for domestic, irrigation and fire suppression systems shall be installed with the proper backflow prevention facilities. The minimum backflow prevention device required for this type of application shall be a double check valve.

PW8. All water systems shall be publicly owned up to and through the water meter. For fire mains, the City shall own up to and including the Post Indicator Valve. All water mains and fire hydrants shall be located in public right-of-way or easements dedicated to the City of Poulsbo. Dedicated water lines shall be centered in an easement of 15 feet in width.

PW9. Domestic and fire flow may share the same water line. The domestic service must exit the water line before the fire service.

PW10. A double check valve assembly shall be installed within 18-inches of the downstream side of the water meter. The double check valve assembly shall be tested by a "city approved" state certified tester upon installation. A copy of the test report must be sent to the Public Works and Engineering Departments.

PW11. Individual PRVs may be required for each metered connection if the pressure exceeds 80 psi.

PW12. Water valves are required on all legs of tees.

PW13. All hydrants shall have bollards installed.

Irrigation

PW14. Irrigation water shall come from a separate connection. Please show irrigation connection(s) on the utility drawing(s).

PW15. A double check valve assembly shall be installed within 18-inches of the downstream side of the water meter.

PW16. The double check valve assembly shall be tested by a “city approved” state certified tester upon installation. A copy of the test report must be sent to the Public Works and Engineering Departments.

Sewer

PW17. All building shall be connected to City

PW18. Sewer general facility charges fees are based on water meter size.

PW19. Service connection to the City sewer system shall be the responsibility of the property owner and shall comply with state and local design and development standards.

PW20. Waste water discharges from the proposed development into the City of Poulsbo’s sanitary sewer system shall meet the requirements set forth in Section 13.06.340 of the Poulsbo Municipal Code with regard to waste strength and unlawful discharges.

PW21. All manholes will be required to have an insert installed. The insert shall be ‘The Rainstopper’ by Southwestern Packing & Seals, Inc. Further information available upon request from the Public Works Department.

Solid Waste

PW22. Solid waste service for this project will be provided by the City of Poulsbo, recycling will be provided by Bainbridge Disposal.

PW23. A solid waste collection/enclosure area must be located on the property adequate to contain both solid waste and recycling dumpster(s)/container(s). The enclosure location must be situated to provide screening from public access ways, with straight drive into the enclosure, and with a width to provide easy access motions for the collection vehicles. Generally, the following requirements apply:

- i. Solid waste pads for dumpster and recycle totes must be designed to City specifications.
- ii. City solid waste collection vehicles will not back uphill onto city streets.
- iii. If a dumpster is located down an incline in the parking garage, the collection vehicle must be able to exit the building moving forward.
- iv. The minimum clearance height for dumping dumpsters is 21 feet.
- v. The minimum clearance height for maneuvering under an overhang is 16 feet.
- vi. The minimum clearance width for truck access is 9 feet.
- vii. Enclosure gates must swing 135 degrees from closed position for truck to access an enclosure.
- viii. The dumpster pad must be flush with the truck ingress/egress.
- ix. No other uses will be allowed in the solid waste enclosure other than to hold garbage and recycle totes.
- x. City collection staff will not maneuver commercial size solid waste receptacles as this is a common on the job injury cause.
- xi. Care must be taken in the design of the solid waste enclosure to limit the drainage of untreated water to the City’s stormwater system.

General Conditions

PW24. Design: All water, wastewater, stormwater system facilities and streets shall be designed by a professional engineer registered in the State of Washington. Design and installation of the improvements shall be the property owner’s responsibility.

PW25. Design and development shall be subject to but not limited to the following standards:

- City of Poulsbo Utility Comprehensive Plans
- City of Poulsbo Construction Standards and Specifications
- City of Poulsbo Municipal Code
- Washington State Department of Ecology 2019 Stormwater Management Manual for Western Washington
- Washington State Department of Health Design Standards
- Washington State Department of Ecology’s Criteria for Sewage Works Design
- American Public Works Association/Department of Transportation Standard Specifications

PW26. In the event there is a conflict between construction standards, the more restrictive standard shall apply as determined by the City Engineer.

PW27. No walls or structures shall be permitted in utility easements.

PW28. Placement of landscape plantings and/or street trees shall not interfere with utilities. Required landscape vegetation may need to be relocated in the final landscape plan. Landscape vegetation not required by city code may need to be relocated or removed from the final landscape plan.

PW29. Appropriate easements shall be provided for public and private utilities as needed.

PW30. City owned utilities shall be located in right-of-way or easements which are dedicated to the City.

Submittal and Approval

PW31. The applicant shall be required to submit to the City for approval, the plans and specifications associated with design and construction of utility system improvements.

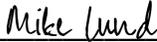
PW32. Utility systems include, but are not limited to, distribution and collection mains, pumping facilities, storage reservoirs, detention/retention facilities or any improvements to be dedicated to the city under a deed of conveyance.

PW33. Upon completion of the project, the developer shall supply the Public Works Department with a copy of drawings of record; these drawings shall be in hard copy form and in electronic form compatible with the most recent version of AutoCAD.

Connection Fees and Assessments

PW34. Utility connection fees are paid based on the current fee at the time of building permit issuance. The connection fee is due at the time of building permit. Early payment and reservation of water connection are not provided for in Poulsbo Municipal Code.

Signed by:



Mike Lund, 4FD1FE3DA34C467...

Public Works Superintendent

4/16/2025

Date

Following are the Poulsbo Fire Department's Conditions of Approval:

- F1. Due to the size and the intended use of the building, S-1 Occupancy (auto repair), a fully automatic NFPA 13 fire sprinkler system shall be installed throughout the building.
- F2. Fire hydrant, Fire department connection and Post indicator valve need to be located within 25 feet of each other. FDC is to be located no closer than 10 feet nor farther than 100 feet from the building.
- F3. The Fire Department prefers outside access to the riser room. If outside access is provided the PIV can be eliminated.
- F4. Full addressable automatic Fire Alarm system reporting to a central station is required under the Poulsbo Municipal Code (PMC).
- F5. The exterior horn strobe shall be installed on the upper 25% of the building away from the annunciator panel. The location shall be approved by the fire department.
- F6. With the installation of the fire alarm system and fire sprinkler system, a Knox Box, Key box is required to be installed on the building. Contact the fire department for ordering information and to coordinate the installation location on the building.
- F7. Fire Access lanes must be designed to support 75,000 lbs and are all weather.
- F8. All Fire Access lanes must be at least 20' wide and any gates must also provide 20' opening (see 19' gate on Vetter).
- F9. No Parking-Fire Lane signage and/or Fire Lane curb markings as appropriate to maintain 20' fire department access.
- F10. Address and suite numbers shall be marked as per the PMC 12.24.060.

Signed by:

 Brian Ramey,
49B00711994242F...
 Captain | Deputy Fire Marshal | Marine
 Program Manager

4/15/2025

 Date